

Maryland Historical Trust

Maryland Inventory of Historic Properties number: AL-I-A-180

Name: FIFTEEN MILE CREEK RD. OVER WHITE SULPHUR
SPRING RUN

The bridge referenced herein was inventoried by the Maryland State Highway Administration as part of the Historic Bridge Inventory, and SHA provided the Trust with eligibility determinations in February 2001. The Trust accepted the Historic Bridge Inventory on April 3, 2001. The bridge received the following determination of eligibility.

MARYLAND HISTORICAL TRUST	
Eligibility Recommended _____	Eligibility Not Recommended <u>X</u>
Criteria: <u> </u> A <u> </u> B <u>X</u> C <u> </u> D Considerations: <u> </u> A <u> </u> B <u> </u> C <u> </u> D <u> </u> E <u> </u> F <u> </u> G <u> </u> None	
Comments: _____ _____ _____	
Reviewer, OPS: <u>Anne E. Bruder</u>	Date: <u>3 April 2001</u>
Reviewer, NR Program: <u>Peter E. Kurtze</u>	Date: <u>3 April 2001</u>

guy

Maryland Inventory of Historic Properties
Historic Bridge Inventory
Maryland State Highway Administration
Maryland Historical Trust

MHT Number AL-I-A-080

Name and SHA No. Fifteen Mile Creek Road over White Sulphur Run/A10900 (A10910)

Location:

Street/Road Name and Number: Fifteen Mile Creek Road

City/Town: Pratt Vicinity x

County: Allegany

Ownership: State x County Municipal Other

This bridge projects over: Road Railway x Water Land

Is the bridge located within a designated district: yes x no

NR listed district NR determined eligible district

locally designated other

Name of District _____

Bridge Type:

Timber Bridge

Beam Bridge Truss-Covered Trestle

Timber-and-Concrete

Stone Arch

Metal Truss

Movable Bridge

Swing

Bascule Single Leaf Bascule Multiple Leaf

Vertical Lift Retractable Pontoon

x Metal Girder

x Rolled Girder Rolled Girder Concrete Encased

Plate Girder Plate Girder Concrete Encased

☐ Metal Suspension

☐ Metal Arch

☐ Metal Cantilever

☐ Concrete

☐ Concrete Arch ☐ Concrete Slab ☐ Concrete Beam

☐ Rigid Frame

☐ Other Type Name _____

Description:

Describe Setting: A10900 (A10910) carries Fifteen Mile Creek Road over White Sulpher Run in Allegany County, Maryland. Fifteen Mile Creek Road runs generally north-south at this location; White Sulpher Run flows east-west. The bridge is located in a wooded rural area with no structures in view. The bridge is located within Green Ridge Forest.

Describe Superstructure and Substructure: A10900 (A10910) is double span steel stringer bridge with a timber deck. The timber planks are supported by timber floor beams, and both of these timber elements are supported by the steel stringers. There is a two strand steel bridge railing on either side of the deck. Each span length is 43' and the total bridge length is 89'. The substructure is two stone masonry abutments and one stone masonry pier. The abutments are listed in fair condition, with some cracking and spalling. No major repairs were recommended in the 1993 inspection report.

Discuss Major Alterations: A10900 (A10910) was reconstructed in 1959. The deck and steel beams were replaced, and it is likely that repairs were made to the abutments. The 1993 inspection report recommended replacement of the timber deck, and it is slated to be replaced in the Summer of 1998.

History:

When Built: 1935

Why Built: local transportation needs

Who Built:

Why Altered: structural and safety improvements

Was this bridge built as part of an organized bridge building campaign: yes

Surveyor Analysis:

This bridge may have NR significance for association with:

☐ A Events ☐ Person

☐ C Engineering/Architectural

Was this bridge constructed in response to significant events in Maryland or local history: no

When the bridge was built and/or given a major alteration, did it have a significant impact on the growth and development of the area:no

Is the bridge located in an area which may be eligible for historic designation and would the bridge add to or detract from historic and visual character of the possible district:no

Is the bridge a significant example of its type:no

Does the bridge retain integrity of the important elements described in the Context Addendum:The steel beams (a primary CDE) and the deck (a secondary CDE) were replaced in 1959. There have probably been modifications made to the abutments (a primary CDE) as well. These extensive alterations raise doubts about the integrity of A10900 (A10910).

Is the bridge a significant example of the work of the manufacturer, designer, and/or engineer and why:no

Should this bridge be given further study before significance analysis is made and why:Further study is not warranted for A10900 (A10910) because of the extensive modifications made to the structure in 1959.

Bibliography:

Allegany County

v.d. Bridge Inspection Files

Greiner, Inc.

1995 Historic Bridge Inventory Form

Spero, P.A.C. & Company, and Louis Berger & Associates

1994 Historic Bridges in Maryland: Historic Bridge Context

United States Geological Survey

1950, 7.5' Artemas Quadrangle, photorevised 1974

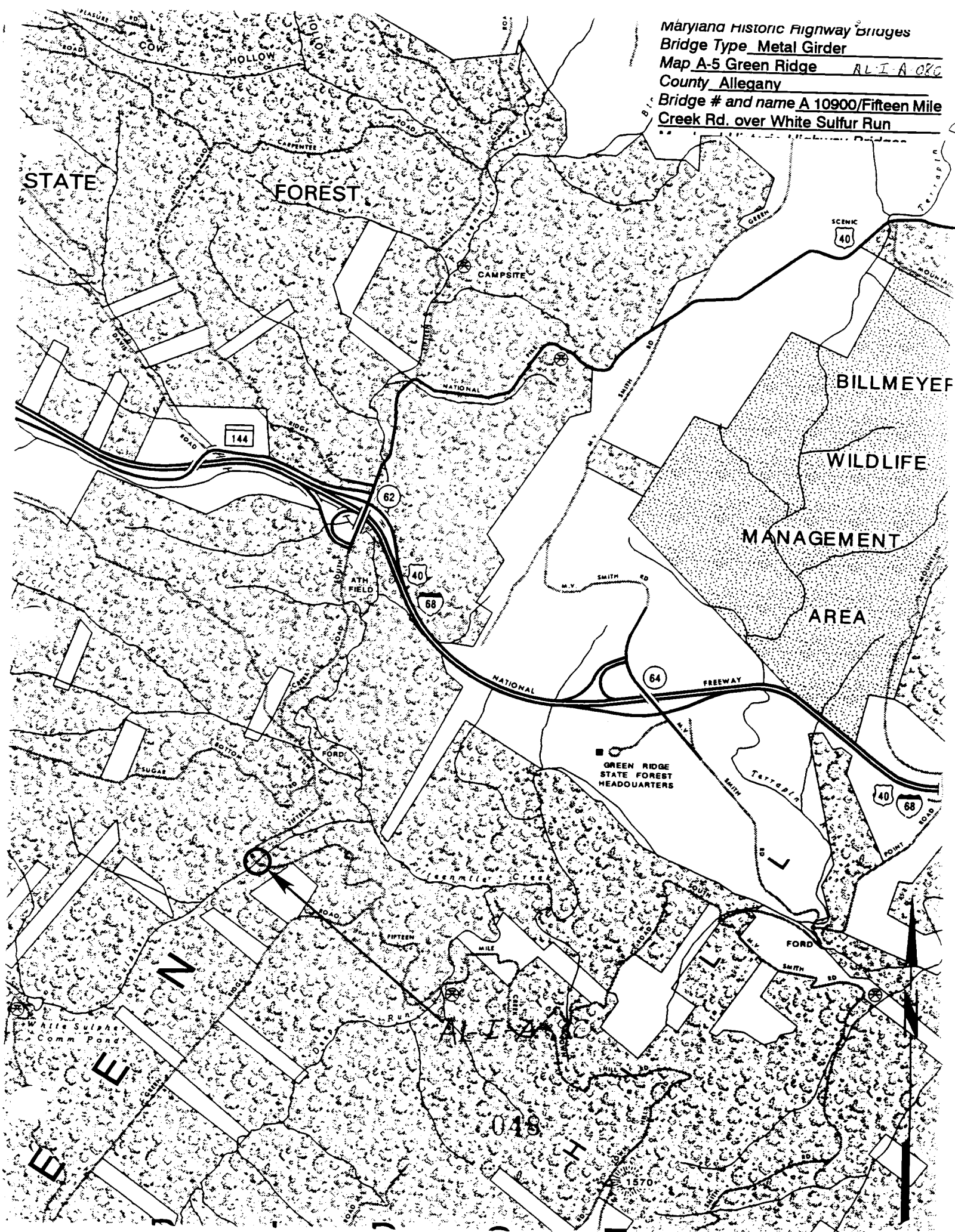
Surveyor:

Name: Stephanie L. Bandy **Date:** September 1995

Organization: State Highway Admin. **Telephone:** (410) 321-2213

Address: 2323 West Joppa Road Brooklandville, MD 21022

Maryland Historic Highway Bridges
Bridge Type Metal Girder
Map A-5 Green Ridge ALI-A ORG
County Allegany
Bridge # and name A 10900/Fifteen Mile
Creek Rd. over White Sulfur Run





AL-IA-080 BR#20A10910 (A10900)

OVER WHITE SULPHUR RUN

ALLEGANY CO., MD.

DAVID P KING

2/3/95

S. H. A.

NORTHEAST ELEVATION

1 OF 4



WEIGHT
LIMIT
6
TONS

AL-I-A-080

BE # 20A10910 (A 10900)

OVER WHITE SULPHUR RUN

ALLEGANY CO. MD

DAVID KING

2/3/95

S. H. A.

SOUTHEAST APPROACH

2 OF 4



AL-I-A-080

BR # 20A10910 (A 10900)

OVER WHITE SULPHUR RUN

ALLEGANY CO. MD.

DAVID KING

2/3/95

S. H. A.

NORTHWEST APPROACH

3 OF 4



AL-I-A-080

BR # 20A10910 (A 10900)

OVER WHITE SULPHUR RUN

ALLEGANY CO. MD.

DAVID KING

2/3/95

S. H. A.

SOUTHWEST ELEVATION

4 OF 4